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'HER WAY' RECOMMENDATION REPORT

Supporting women in Canberra's south

who want to make greater use of public transport and active travel



Authorised by Dr M Paterson MLA for ACT Labor

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Introduction

Why 'Her Way'?



'Her Way' is a campaign about travel behaviour, to support women in my electorate, and from Canberra's southside, who want to change their habits and adopt different modes of transportation that have a range of flow on benefits for personal health and the environment.

I want to support people to make transport choices appropriate to their situation that will improve their daily routines and ease the stress of their daily commute.

I particularly want to work with those who have an interest in transitioning away from private car use, but might be concerned about it, or don't quite know where to start.

The reason that I have specifically targeted women in the 'Her Way' campaign is because studies suggest that women are an 'indicator' species for the safety and uptake of public transport (PT) and active travel (AT) in any city.

That is, the higher the number of women engaging with PT and AT, the more effective and safer the travel system for everyone. Although society is shifting, studies show that – on average – women still carry the bulk of the childcare responsibility and household labour. So, basing the campaign around women who potentially face substantial challenges with time and flexibility will ultimately ensure that PT and AT will be more accessible for everyone.

I believe that if policies, programs, designs and infrastructure are targeted to reach those who face substantial barriers in engagement with different transport options, we will also be helping anyone across the ACT who wishes to incorporate more regular use of PT and AT in their regular routines.

A 2018 report by the International Transport Forum notes that “women tend to have specific travel characteristics in terms of mode choice, time of travel, trip purpose, route, trip chain and travel distance, and they have very different preferences and constraints than men”.

The timing of the 'Her Way' campaign is coincided to work with people to find ways to minimise the inconvenience and disruption caused by the construction of light rail stage 2. The ACT Government has recently commenced work to deliver this large-scale infrastructure project which will result in an extension of the network to Woden.

Research shows that the best time to create new behaviour is at a time of disruption – whether this be a new job, a new home or other significant life milestones. Returning to a degree of normality after the covid-lockdown presents such an opportunity; as does the disruption and commencement of light rail.

This Recommendation Report complements the many strategic policies, regulations and legislation of the ACT Government relevant to reducing transport emissions, addressing climate change, having a positive impact on physical and mental health and wellbeing, and creating a livable city based on sound place-making principles.

Many recommendations contained in this report are targeted at office-based workers. However, the recommendations will positively impact users of all modes of transport, and across different times of the day and week. In addition, some specific recommendations target vulnerable and disadvantaged groups within our community, including people living with a disability and people of low socio-economic demographics.

I note there is much more needs to be done to provide transport solutions for all people in our community, including diverse groups – such as those who face vulnerability or disadvantage, older Canberrans, those outside the workforce and accessing income support, and people in situations of precarious and casual employment.

This should form a basic starting point and premise for all transport discussions, policies and outcomes and is a matter that warrants dedicated attention beyond the limitations of this campaign.

While much of this Recommendation Report focuses on journeys to work, initiatives and actions should also target the use of walking, riding bikes and PT for short, local trips. Many

respondents of the 'Her Way' survey indicated their interest in switching transport modes for journeys of social purposes or to the local shops.

Many of the initiatives outlined through this Recommendation Report are relevant to short journeys, and could benefit from specific, targeted communications and promotional campaigns to encourage that behaviour.

Key principles

Key and guiding principles for the 'Her Way' campaign and this Recommendation Report, based on global best-practice active travel methodologies and expertise, include:

- ensuring a data-driven and evidence-based approach to interventions, messaging, campaigns and activity;
- focusing not on the bike or public transport *per se* but on the type of city in which we want to live and the type of lifestyle we wish to have;
- providing a quality of experience by walking, bike riding and public transport – ensuring ambience and amenity of the environments passed through (and of which safety is a critical contributing factor);
- ensuring appropriate language and messaging throughout any media, marketing or promotional activities about active travel and public transport - including from key influencers;
- ensuring that public perception is not about 'something being taken away' or 'being given to someone else' (i.e around road usage) but that an opportunity is being provided;
- being willing to take low-cost risks, trials, pilots and experiments;
- reducing the perceived fear and reluctance of change by those who will remain averse to increased bike-riding, walking and public transport.
- continuing to ensure urban planning and transport policy are integrated to provide positive shared space outcomes which prioritise amenity and safety for walking, riding bikes and catching PT.



Methodology

I released a Discussion Paper on 9 August 2021 outlining the issues I was wanting to explore and seeking input from women in the ACT, particularly those living southside, through an online survey.

Participants were asked to provide input about their views on constraints, barriers and opportunities regarding different modes of transport.

The survey asked about respondents' current modes of transport, their preferred modes of transport, their travel behaviour (for example, trip-chaining requirements), barriers and constraints to different modes of transport, and flexible working arrangements. Questions about flexible working arrangements are relevant to modes of transport as they can impact on the need for travel, the times of day of travel and other transport-related barriers, constraints and opportunities.

Feedback on the Discussion Paper was received via:

- an online survey which was hosted on my website and promoted through local media channels and by key stakeholder groups (84 surveys completed);
- social media platforms; and
- by email.

The survey was available over a period of four weeks.

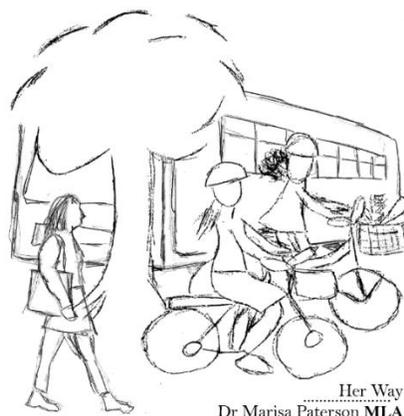
I also met with a range of stakeholder groups during – and beyond – the public consultation period to obtain a better understanding of the possibilities and constraints – including from different sectors within the community, programs already in place, and best practice approaches that might be applicable for the ACT. Stakeholders included:

- Pedal Power ACT
- Amy Gillett Foundation
- ACTCOSS
- Aus Cycling
- Conservation Council ACT
- Early Childhood Australia (ACT)
- Heart Foundation ACT
- Physical Activity Foundation
- Women's Health Matters
- YWCA

In undertaking the analysis to inform this report, I note that the results are biased, reflective of respondents who opted to take part – indicating some level of interest in making changes to their travel patterns and behaviour.

A copy of the Discussion Paper is available at:

<https://www.marisapaterson.com.au/news/campaigns/her-way/>
<https://www.marisapaterson.com.au/news/campaigns/her-way/>



Summary of community comment and input

Most women who responded to the 'Her Way' survey (60%) drove a car to get to and from work. An even higher percentage (71%) drove a car for short journeys – social, recreational and shopping purposes.

Most respondents who indicated they want to make changes to their modes of transport want to do so for journeys to work (38%). Many are also interested in making changes for journeys for recreational, sports or social purposes (27%), and for journeys to local shops (24%).

Only a small number of respondents (12%) are interested in making travel changes for journeys to take children to or from school.

Of those wanting to make changes to their modes of travel, riding a bike is the most popular preferred mode (40%), followed closely by public transport (37%).

Most respondents work full time, have access to flexible working arrangements and utilise those arrangements. Despite this, many other respondents also indicated their interest in working more flexibly, including the ability to work part-time from home and working more flexible hours.

The full results of quantitative and qualitative input are provided at Appendix A.

In the responses provided, it is clear that choices about travel behaviour and mode of transport are not made in isolation, and are heavily influenced by other matters including:

- time pressures;
- household logistics (school/childcare drop-off and pickup, flexible working arrangements, and other activities including groceries);
- personal safety;
- quality and enjoyment of life;
- seasons/weather; and
- financial considerations.

Health, sustainability and environmental considerations were not noted by respondents as a significant contributing factor to transport decisions.

Transport for commuting purposes (whether to work, school, the shops, or for social, recreational or other purposes) is mostly a means of simply getting from A to B in the fastest, most convenient, safest way possible.

Time is THE single most valuable commodity amongst people who provided input on 'Her Way'.

Respondents are keen to make changes to their travel behaviours, but only if it doesn't create a significant impingement on their time (and quality of life):

Every minute of every day is planned and precious. We have no choice but to choose the most efficient way to do everything.

Any change in travel behaviour from what is currently considered to be the fastest, most convenient, and safest mode possible must present an option that is more viable and more attractive. If it can't compete on one of these criteria, it must be very appealing on others. Private vehicle use in a city like Canberra – with an excellent reputation as an easy city in which to travel by private car – presents a tough challenge, even in the context of light rail disruption.

PT and AT need to be perceived as the best option for people, taking into account all of their needs – of which 'time' is the most critical but also the most challenging to overcome, together with personal safety and trip-chaining logistics.



'Trip-chaining' is another key consideration in women's transport choices. Trip-chaining refers to travel that involves a sequence or series of activities – dropping children to school and childcare, buying groceries and other errands.

The ACTs 2017 Household Travel Survey notes that 23% of journeys between home and work include trip-chaining.

Barriers associated with trip-chaining include the need to travel to multiple destinations and the need to carry things and/or people/dependents. While trip-chaining isn't addressed as a specific section within this Recommendation Report, many of the recommendations outlined in this report address barriers and constraints associated with trip-chaining. This was identified as a significant barrier and challenge for women in their engagement with PT and AT.



Personal safety is another key consideration when choosing a mode of transport. These comments are woven into every aspect of barriers and constraints, as well as the recommendations throughout this report.

According to 'Her Way' respondents, financial costs also play some part in determining women's travel behaviour. Some respondents suggested incentives to use transport other than a private vehicle for journeys to and from work including:

An option might be to include travel time into your paid workday for people who use green transport - I can drive to work in 20-25 minutes but if I bus at non-peak times (when I can sit) it will take closer to an hour. It just isn't feasible.

People with primary school age children should get parking subsidies - it is almost impossible to combine school drop off pick up with public transport.

Public transport in Canberra is much too expensive for adults without concessions. It shouldn't be about income/concession, but about incentivising the use of public transport. I'm not going to use the bus, especially in conjunction with riding part way, if it's similarly priced to driving my car. If it were cheaper I would ride a bike part way and use the bus the rest, but it currently isn't worth the time, effort or cost to do so, and unless it can come down below \$3 a day (return) it will remain too expensive.

There should be incentives to catch a bus on rainy days if you usually ride.

One respondent noted that travel options are a class-issue and that there are many people across the ACT community for whom PT is not affordable, let alone the luxury of being able to purchase a private car, bike or scooter.

It was also noted that many low-income earners often have the least choice about their transport options because they work multiple casual jobs across different locations, they are shift-workers, or they might be domestic or commercial cleaners who need to transport bulky goods with them throughout the day. Their schedules may also be less structured and more uncertain on a day-to-day basis, making any advance-planning difficult.

Recommendations

Resulting from the analysis of comments received on the ‘Her Way’ community and stakeholder input, there are a number of recommendations that can be developed and implemented in both the short and long-term, to bring about lasting, meaningful change to assist those who wish to make changes to their travel patterns and behaviour.

The key opportunities that will best address the barriers and constraints highlighted by respondents to the ‘Her Way’ Discussion Paper have been broken into three sections: communications, infrastructure and creating the ecosystem, and are provided below.

Disclaimer:

The recommendations in this report have been developed by me, in my capacity as a private member of the ACTs Legislative Assembly. I present the recommendations to the ACT Government for their review and consideration, to incorporate – as appropriate – into existing and future programs of work. I will seek to work with my Ministerial colleagues on the implementation of the recommendations as they see fit, as well as with the community and stakeholder groups.

Communications

Develop a ‘Her Way’ focus group

Many respondents to the ‘Her Way’ Discussion Paper noted their desire to change their travel behaviour and rely less on a private car, but that they’re nervous about it. Many existing stakeholder programs, and case studies nationally and internationally, have shown that buddy-systems work effectively in this situation.

Throughout the development of the ‘Her Way’ Discussion Paper I have said that I’m at the start of my own journey to make better use of PT and AT, and I can relate to those who are nervous about starting this journey.



I’m intending to develop a small focus group to work closely together, to support and learn from each other, and share our stories to help inspire others.

I envisage the group will comprise some of those who provided comments through the online ‘Her Way’ survey or via email and will meet regularly. I have already sought support from key stakeholder groups to meet with the focus group and share insights, experience, advice and support.

Following the public release of this Recommendation Report I will seek to establish this group.

In addition, I will continue the conversation I’ve started with Early Childcare Australia (ACT) and will further engage with childcare centres in my electorate to understand more about the logistics involving childcare drop-off and pick-up. I’m keen to hear from parents about ways to ease this aspect of their lives and make it more convenient and less pressured by time and travel constraints.

To this end, I intend to undertake the following:

1. Establish a women’s southside focus group to:
 - a. support each other to change our travel behaviour
 - b. document our journeys, share our stories and inspire others
 - c. engage with and learn from key stakeholder groups
2. Engage with childcare centres and the ACT Council of Parents and Citizens Associations to explore opportunities to integrate child drop-off and pick up with PT and AT

Personalised travel plans

Many respondents have indicated the biggest challenge in switching from private vehicle use to PT and AT is knowing where to start. The survey findings highlighted the complexity of people's everyday lives, and an individual's personal challenge with trying to understand the PT and/or AT networks to get themselves from A to B. Respondents highlighted the need for a point of contact to talk to about an individual's circumstances and their personal travel needs and provide tailored route advice.



Regarding trip-chaining, there was much commentary – particularly on social media – about the roles and responsibilities within households, and the correlating options for travel choice. Some felt that women bear the vast responsibility of trip-chaining activities and, as a result, it makes greater sense for them to have the convenience of driving a private vehicle. Others felt that roles and responsibilities are shared within households. It was clear in all comments that trip-chaining, regardless of who in the household has primary responsibility for it, creates challenges for using PT and AT.

One respondent considered that individuals might be able to re-think their situation to better assess their transport options with regard to trip-chaining, including the following suggestions:

- *Number one tip that any productivity consultant is going to give you is that you can reduce your workload by delegating tasks or simply removing tasks from your list.*
- *Do you really need to drive 12yo kids to school? Can't they walk, ride or bus themselves?*
- *Does it really need to be you transporting the kids? What about arranging a round robin with parents of your child's friends in the neighbourhood?*

I recommend that resources, information and journey planning tools are provided to support residents in developing personalised PT and AT travel plans, including multi-modal options.

Personalised travel plans could help address concerns raised by respondents about needing to be flexible and available to collect children during the working day, should an emergency arise. A Plan B might be able to be established, in conjunction with the expertise and knowledge of a 'personalised travel plan consultant,' for these circumstances. Plans could be developed for individuals, or for households.

Dedicated resourcing to develop personalised travel plans might be implemented initially to support women in Canberra's south, in response to the upcoming disruption of light rail and then more broadly across the ACT.

Personal travel plans might include goals, check-ins and six-week plans, similar to programs established by the Heart Foundation. This has proven to be effective in creating a level of commitment and self-accountability by participants to incorporate more walking into their regular routines.

Over the longer term, digital technology solutions might also be incorporated to support people with their personalised travel plan.

RECOMMENDATIONS:

1. Provide resources and tools to support people to prepare personalised PT and AT journey planning, including multi-modal options
2. Engage with businesses to encourage them to have discussions with their employees to develop personal travel plans

Digital technology

A number of respondents reported that they would like improved digital and real-time information for PT services, noting this would go some way to addressing concerns of personal safety (minimise time waiting at bus stops), and household scheduling, by limiting time wasted in waiting at bus stops or interchanges.

Several respondents noted that a physical lack of, or a lack of awareness of the location of, secure bike parking facilities in public places – local shops and the like – was a deterrent to riding. This was especially noted as a challenge for expensive e-bikes. Respondents suggested that some mechanisms to overcome this barrier might include a readily available map of the location of secure bike parking facilities, and to ensure the placement of bike-parking in areas of high passive-surveillance.



The ACT Government already has real-time PT scheduling available through a smartphone app. In addition, google maps and other platforms, including the ACT Government’s own journey planner, enable ACT residents to plan their journeys, including use of PT and AT, though with some limitations. Information about safety features and safe routes could be incorporated in the app, including preferred routes in the dark.

I recommend that these features be further promoted to the community to raise further awareness about their availability and use.

I also recommend that every opportunity be sought to ensure that ACT Government journey planning apps and online platforms remain accurate, up to date, user-friendly and convenient.

The locations of secure bike parking facilities across the ACT should also be incorporated into these platforms.

RECOMMENDATIONS:

3. Improve communication about existing digital technology apps which provide real-time PT updates
4. Develop digital technology apps which integrate route planning, bike parking locations, real-time PT scheduling and MaaS

Marketing campaign

Survey respondents and consultation identified that, often, women’s perception of themselves is at odds with perceptions of those who engage in AT, and in particular with ‘cyclists’ (i.e. they do not see themselves as lycra clad and super-fit). Much can be done to normalise these modes of transport and change that narrative and perception through a dedicated campaign. Similar effective campaigns have been developed in recent years in the UK, New Zealand and Sydney, among others.

There are many existing reference sites for information about AT and PT including the ACT Government’s Transport Canberra sites and those of stakeholder organisations. However, they require awareness by the user of the different stakeholder groups.

I recommend that a single webpage be developed, dedicated to women who want to change their travel behaviour with information that would directly respond to what women said they want to know. This dedicated and targeted reference point will help address some of the comments requesting more readily available access to information including:

- where to ask for help in selecting a bike
- how to learn more about basic bike mechanics including changing a tire
- information about how to plan journeys by PT and AT
- helping to build confidence in using modes of travel other than personal cars

The language we use can go a long way to help humanise and normalise different modes of transport. Labels such as 'cyclists' can be counter-productive to shared-road/path cohesion, with preferred terminology being: people who drive cars, people who catch PT, people who ride bikes or scooters and people who walk.

RECOMMENDATIONS:

5. Design and conduct a dedicated marketing and communications campaign, to coincide with the disruption of light rail stage 2 construction, encouraging the use of PT and AT, and normalising this type of behaviour
6. Ensure language and imagery is relevant to the audience
7. Develop a single, dedicated website to directly respond to the concerns of women who want to better use PT and AT but don't know where to start
8. Explore opportunity to incentivise people to switch to increased use of PT and AT

Infrastructure

Active travel network and bike riding

Many respondents commented on AT infrastructure across Canberra, noting that 'Canberra can be the best cycling city in the world'!

However, suggestions from respondents for improvement to AT infrastructure (including for people using the shared paths to walk as well as ride) include:

- More separated bike lanes;
- Greater access east-west/ Tuggeranong/Fyshwick;
- Improvements in connectivity to Barton, Woden Town Centre, Garran and Yarralumla;
- Improved shared path connectivity;
- Priority crossings; and
- Enhanced path maintenance and repair.

Some respondents noted that existing bike infrastructure is not ideal, particularly if riding with children – either on their own bikes or in a bike-trailer or cargo-bike. These respondents noted the need for path widening and curb ramps. Similar comments were made by respondents who would like to see improvements for pedestrians.



Whether walking or riding a bike, many respondents noted the need to make the shared path network safer through increased lighting.

A range of matters were noted by respondent as preventing them from riding a bike, primary of which was the distance they needed to cover – that it was too far to ride.

A small number of respondents expressed concern about personal safety when riding a bike, especially riding on the road, while others noted that they needed help selecting a bike, choosing a route and knowing how to fix a punctured tire. One person cited lack of fitness as a barrier, another cited the weather – too hot in summer and too cold in winter, and another noted that magpies are a deterrent in spring.



One respondent noted that mandatory helmet laws are a deterrent, stating that, not only does it lead to unflattering ‘helmet hair’, but also:

it’s inconvenient to lug a helmet with you at shops etc.

I appreciate the safety considerations, but I do feel that on a gentle bike path commute or short trips on flat terrain it could be discretionary, as it is in every country in Europe.

The culture and use of road space (including shared paths) was noted by some respondents as a factor in their experience and consideration of transport modes.

These respondents noted a need for separation between different users of all modes of transport – pedestrians, bikes/scooters and motorised vehicles. Some respondents suggested that transport infrastructure should give greater prioritisation to people walking and riding bikes, and that safer street/shared road principles should be applied across the ACT.

One respondent noted that she believes walking is the least prioritised mode of transport in the ACT:

Footpaths are not well-maintained, or non-existent, and there are many busy roads with very few safe places to cross.

It was also noted that more needs to be done to foster a shared sense of respect, courtesy and consideration among all road and path users:

Reckless, inconsiderate and/or abusive cyclists who verbally threaten to run me down as a pedestrian on the shared path make me feel unsafe.

A generalisation but drivers’ attitudes to bike riders is appalling and very dangerous.

Significant investment is continually being provided by the ACT Government in its commitment toward this and the 2021-22 ACT Budget has made significant commitment for AT. Prioritised works help to ensure that funding and resources are allocated where they will have the greatest positive impact and will benefit the greatest number of people.

RECOMMENDATIONS:

9. Trial pop-up separated infrastructure in appropriate locations using low cost, minimal intervention techniques
10. Investigate options to address missing links or safety gaps in AT connections between the southside and the city
11. Provide safe cycling routes along the light rail corridor
12. Investigate the viability of direct safe cycling infrastructure connecting areas of Murrumbidgee and the existing shared path network with Fyshwick and Tuggeranong
13. Continue maintenance and repair of footpaths, and continuation of the Government’s ‘Age Friendly Suburbs’ project

Bike parking

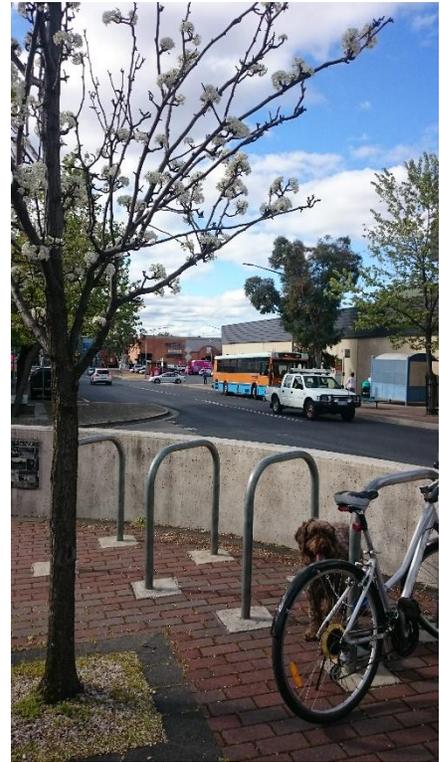
As noted above, several respondents noted that a physical lack of, or a lack of awareness of the location of, secure bike parking facilities in public places is a deterrent to riding.

One immediate, short-term action that could be taken to support more women to ride bikes is the development and promotion of a map – in both print and digital formats – which clearly identifies the location of bike parking facilities across the ACT. Perhaps a grant might be available to assist a stakeholder group to undertake this body of work.

Longer term, a review should be undertaken of all existing bike parking locations across the ACT, ensuring they are placed in areas of high passive surveillance. Requirement should also be placed in all design standards and infrastructure delivery to ensure that all future bike parking facilities are located in areas of high passive surveillance. I recommend that those with lived experience have input into the location of bike parking facilities – to ensure that bike parking facilities are convenient for people riding bikes, and also do not hinder other users of a space including movement patterns of those with mobility aids.

RECOMMENDATIONS:

14. Develop print and digital maps indicating the location of bike-parking facilities across the ACT
15. Ensure existing and new bike parking infrastructure is located in areas of high passive surveillance and that they provide appropriate security for expensive e-bikes - Work being undertaken at the Woden interchange should reflect this approach
16. Undertake a review of bike cages across Canberra and the existing pass system, including their supply and demand, to explore options for greater use
17. Include representatives with lived experience in decisions about the locations of bike parking facilities



End of trip facilities

Many respondents noted workplace arrangements and a lack of end of trip facilities as a barrier to AT:

I need easy access to showers at my workplace.

There should be requirements for all buildings to have end of trip facilities for cyclists

The ACT Government's draft End of Trip Facilities (EoT) General Code, together with associated guidelines, are in the final stages of community consultation. These documents should be finalised as soon as possible and communicated to developers and workplaces across the ACT. Guidelines should address options for retrofitting existing buildings with end of trip facilities.



These documents, and any other related materials should note the importance of incorporating user perspectives and lived experience in the design and construction of end of trip facilities.

Grant opportunities might be considered to assist workplaces to retrofit functional secure bike parking, change rooms, showers and lockers in buildings (perhaps similar to the Building Energy Efficiency Upgrade Fund).

Education and training services should be provided for facilities managers and others with responsibility for encouraging the use of PT and AT to help them understand and meet user needs.

This will ensure that facilities are functional – and even enticing – for users and would-be users. Small details from a user perspective can make a big difference to the viability of end of trip facilities, including bike parking (both location and design), change room location and design, locker size and functionality, etc.

The ACT Government could consider supporting or facilitating partnership arrangements between workplaces who cannot provide EoT facilities for their staff and other businesses or work sites who could provide these facilities, such as gyms or other nearby buildings with EoT facilities.

RECOMMENDATIONS:

18. Finalise the ACT's End of Trip Facilities General Code and associated guidelines. Ensure these documents include the requirement for lived experience and user perspectives to be incorporated in design and construction
19. Develop best practice guidelines for retrofitting End of Trip Facilities in existing buildings
20. Assist workplaces to develop partnerships to enable staff access to EoT facilities at nearby locations where workplaces cannot provide them; and support collaborative opportunities for pilot projects

Public transport

There was significant commentary by 'Her Way' respondents that current public transport (PT) timetables and routes are not always convenient or conducive to enable respondents to make transport behaviour changes – both for direct trips, and in the context of trip-chaining and needing to take children to and from school and childcare. Part of this issue is that respondents feel they have a long distance to travel for work, and that their travel time can be increased significantly by choosing PT rather than driving a private car.

It appears that many respondents have reviewed their PT options and undertaken basic time-cost calculations to determine that this is not a feasible option for them.

Respondents consider that the increased time-factor in taking buses is primarily related to indirect routes, as well as the time required to access bus-stops and stations (first and last mile issues), and time spent waiting for connecting services at interchanges.

Need improved last-mile PT. If I drive to work, it takes 20-minutes. If I catch the bus, it takes 1:15-hours, mainly due to the walking at each end.

When I do catch an express bus, I still have a fair way to walk to the express bus stop, and I have to do a very long day due to lack of buses on the express route.

One respondent noted her concern taking children on a 'packed commuter bus'.

Added to this, a considerable number of respondents expressed concerns about personal safety when using PT – primarily related to dark bus stops and bus interchanges, as well as the need to walk in the dark – either morning or evening, particularly in winter months – to access PT.



Image courtesy ACT Government

Respondents further indicated that the small financial saving of catching PT compared with driving does not justify the time costs.

When taking into account trip-chaining activities, PT poses additional constraints for respondents. With regard specifically to school drop-off and pick-up, respondents noted:

In the morning I have two school drop-offs... It requires a journey back to the interchange to swap buses. I'd prefer to avoid the interchange with small children in tow. A car is warm, fast, safe for small kids and convenient. I would have to pay more for before and after school care for the time it would take me to use PT.

As a cyclist, I heartily encourage cycling if you can. As a mum and a realist, I know it's impossible when you have kids to drop off at childcare or school.

A key barrier preventing 'Her Way' respondents from adopting greater use of PT is personal safety. Improved lighting should be provided at PT stops and stations, and for popular first and last mile routes.

Secure bike parking at bus stops and stations – as well as for upcoming light rail stops in Canberra's south – can assist in addressing first and last mile issues and related issues of time. Shared scooter schemes can also be further investigated for first and last mile options for Canberra's south.

Current (October 2021) upgrades being undertaken for the Woden interchange and town centre are likely to be welcomed by survey respondents on the basis of the comments provided. This area was noted by many respondents as being of concern and presenting a barrier and hindrance to their use of PT and AT, due to concerns of personal safety. The ACT Government's investment for infrastructure upgrades in Woden will contribute to a much safer experience through improved place-making outcomes, passive surveillance and a more welcoming and attractive environment.

RECOMMENDATIONS:

21. Provide more lighting at bus stops and stations and popular first and last mile routes
22. Ensure light rail stops and terminals have adequate lighting
23. Ensure secure bike parking at bus stops and stations (and for future light rail stations)
24. Explore opportunity to expand the shared scooter scheme in Canberra's south
25. Explore opportunities for increased safety on buses at night-time – eg. more female drivers; more personalised services which take people closer to their home, and/or a public safety initiative similar to 'Ask for Angela' at bars across NSW
26. Consider promoting a buddy system for PT use
27. Continue to work to increase the number of buses having bike racks to give certainty for multi-modal travel

Multi-modal Woden

Some respondents made the following suggestions which would enable them to consider switching from private vehicle use to multi-modal transport:

Additional Park and Ride or Bike and Ride options southside. The closest one I could use is 10km away.

More spaces in the Park and Ride at Woden. By the time I drop my child to school, the Park and Ride is often full.

My kids and I rode into Civic and returned home using the tram earlier this year and I had to be separate from 2 of my kids because all of our bikes couldn't fit in the one bike area on the tram. It's not a big deal but being 25ms away from a 5 yrs old on a moving tram is somewhat disruptive to other commuters. Also, if people are not confident getting bikes onto trams or are sensitive to disrupting others, this may be a barrier to mixed mode travel.

I am also really keen to try using the bus and taking my bike along for the ride. Wish this was free and also am not confident putting my bike on the buses bike rack.

I've tried leaving my bike at Garran primary school and catching the bus but it takes FOREVER.

I recommend that existing Park and Ride demand and capacity be reviewed and assessed and, if appropriate, that options be considered for expansion. The Park and Ride at Woden is one location that respondents indicated should be assessed, due to it often being over-subscribed. This has been said to hinder particularly women who are constrained by timing to drop children at school or childcare and are unable to access Park and Ride facilities early in the morning.

In all design considerations for light rail stage 2 it will be critical that first and last mile solutions are provided and that they offer safe, secure, seamless integration for people who choose to walk, ride, or scoot.



Image courtesy ACT Government

RECOMMENDATIONS:

28. Review Park and Ride demand and capacity to ensure it is adequate
29. Incorporate appropriate levels of Park and Ride facilities with light rail stage 2 infrastructure
30. Ensure that first and last mile solutions to enable easy, seamless integration with light rail stage 2 are appropriate. Include user perspectives and lived experience in the design and development of this.

Creating the ecosystem

Stakeholder partnerships

There are a range of active, expert stakeholder groups across the ACT and nationally who have an interest and ability to help Canberrans transition away from personal vehicle use to greater use of PT and AT. Much of the work undertaken by these groups addresses many of the barriers and constraints outlined by 'Her Way' respondents.

Wherever possible, the ACT Government should embrace partnership opportunities with key stakeholder groups, to work together for a coordinated and consistent approach to encourage adoption of travel modes other than private vehicles.

Stakeholder groups in the ACT and nationally already have many excellent and successful programs in place to support females to ride bikes, which can be further leveraged, supported and promoted by the ACT Government. Some of these include 'She Rides', 'Wheel Women' and 'Girls on Bikes ACT'. These programs help women develop skills, fitness and confidence in a supportive and non-threatening environment.

Some of these programs also include opportunities for 'ride-buddies', where a confident bike-rider will ride with a beginner, or in a small group.



ACT Grants funding programs can seek to make increased use of existing groups and organisations who work in this space. This can be achieved through existing grants programs, and through the facilitation of new grants funding opportunities which might consider specific targets to assist those who will otherwise be impacted during the construction works for light rail stage 2.

Other community engagement activities which can be coordinated with stakeholder partners include 'come and try' days – introducing people to a range of different types of bikes (which might include e-bikes and cargo-bikes); assisting people to manually operate the bike racks on buses, among other activities; leveraging existing stakeholder programs including basic bike maintenance courses; and opportunity to trial ciclovias (where a street is closed to vehicular traffic for a few hours on a Sunday morning for example, as a street festival, encouraging walking and bike riding, and providing a glimpse into what this might look like for further adoption).

RECOMMENDATIONS:

31. Continue collaborative efforts with stakeholder groups for a coordinated approach to encourage Canberrans away from private vehicle use
32. Explore opportunities to assist stakeholder groups to expand and extend existing programs helping Canberrans ride bikes
33. Cyclovia/temporary street closures to cars as part of the ACTs festival calendar

Encouraging the use of e-bikes

Several 'Her Way' respondents indicated their interest in the use of e-bikes to help overcome barriers associated with fitness, distance and hills. These women also noted challenges in adopting greater use of e-bikes including cost, access to charging facilities and lack of, or lack of awareness of, secure bike parking.



Current pricing and Australian legislation (preventing the import of certain e-bikes with increased power and speed) were noted as constraints to purchase. One respondent noted that more charging points across the city might encourage greater uptake of e-bikes.

Another respondent called for incentive schemes and subsidies to purchase electric (or electric assist) bikes. Nationally and globally, demand for e-bikes is growing. This is illustrated through the ACT Government's own initiative of the electric bike library, where there is an ever-growing wait list to access loan of an e-bike for a week. E-bikes help to address issues of distance and fitness, of the need to carry things, as well as countering the need for end of trip facilities. However, they are expensive, with average e-bikes in Australia costing upwards from \$1,500.

Some e-bikes are designed as cargo-bikes or having other seats capable of carrying children. E-bikes are also designed with capacity to carry panniers and bags; and with basket capacity. These types of e-bikes are popular in countries with high cycling participation rates, especially for women, to assist them with trip-chaining activities.

The ACT Government has in place a salary-sacrifice program for e-bike purchase, and an e-bike fleet for staff use for work purposes. I recommend that these programs be reviewed with consideration for further promotion and/or how they might be promoted to other workplaces. It is positive to see the ACT Government lead by example in this space.

I also recommend that the opportunity be explored to expand the e-bike library including with a base on the Southside.

I recommend that investigation be undertaken to install e-bike charging stations in town and group centres to help overcome 'range anxiety' associated with e-bike use.

RECOMMENDATIONS:

34. Assess opportunities to encourage and promote e-bike use by ACT residents
35. Expand the e-bike library through the establishment of a Southside library to compliment that already in the north
36. Consider the provision of charging facilities for e-bikes in town and group centres

Flexible workplaces

Flexible working arrangements were considered by some respondents to contribute and correlate to greater choice and flexibility of transport modes. The correlation between flexible work and travel choices includes:

While I can easily do this (60-70% of time back in the office), the reality for me means that if I spend that extra 60 - 70 mins of time per day commuting/parking, I miss out on riding my kids to school and reading my kindy kid a story in the library before school... So while the idea of 'showing your face' in the office is well intended, the result of such policies means that there are missed opportunities for active travel and time with yourself or your kids.

A silver lining of Covid for me has been more flexibility with work hours which gives me more time to actively travel with my kids and more time to spend with kids.

Some barriers to the adoption of more flexible working arrangements included workplace policies, including among APS staff, and the need for greater cultural acceptance of work-from-home opportunities:

Covid has helped but there's still an element of guilt/fear about 'why aren't you in the office today'?

Some respondents noted that they needed to feel more confident or assertive to be able to have a conversation with their manager/employer about more flexible working arrangements:

I think it is a matter of having the conversation with my boss and negotiating a change. I feel this is more about building trust that I will do the work and managing others' expectations regarding my availability.

One respondent noted that 'agency to agency agreements to share office space' would be beneficial for more flexible work and travel/lifestyle choices.



The best way to save travel time is to work at, or closer to, home. Working at home (whether part- or full-time) may suit some but is not ideal for everyone, and/or not all the time. However, having the ability to work *closer* to home addresses many of the barriers to PT and AT modes of transport highlighted in the 'Her Way' community responses. These include reduced travel time; enabling easier trip-chaining logistics; safer, more convenient and direct PT and AT journeys (achieved through less time travelling in the dark at either end of the day); and incorporating some physical activity.

The ACT Government's 2021-22 Budget recognises this opportunity and has committed \$2.5 million over four years to refurbish office space in Tuggeranong and Belconnen for this purpose. I recommend that, at the appropriate time, a review be undertaken, to consider options for the Woden Town Centre and other office buildings in Murrumbidgee.

The ACT is well positioned to take advantage of this concept, with a large public service workforce and dispersed town and group centres across the ACT. There may be further opportunity to develop agency agreements with Federal Government Departments and potentially for other large-scale corporate businesses to partner with each other, enabling employees to access office space closer to home.

In addition to satellite offices, the ACT Government should consider engaging more broadly with workplaces across the ACT to assist them in continuing existing efforts towards a culture of greater flexibility for employees. This might include a promotional and communication campaign to help workplaces understand the benefits of flexible work arrangements; it might include assisting individual workplaces to help them develop flexible working arrangements and associated workplace travel plans for their staff; and/or a range of other initiatives.

RECOMMENDATIONS:

37. Evaluate and review the AT Government's Belconnen and Tuggeranong flexi-hubs, once established as per the 2021-22 Budget, to assess opportunity for flexi-hubs in Murrumbidgee
38. Engage with businesses in the ACT to encourage their employees' to engage with PT and AT travel
39. Work collaboratively in promoting a culture of flexible work across the ACT

Applying an inclusive and diverse lens

The 'Her Way' campaign casts a light on the different transport patterns and behaviour of women than men. Beyond a gendered lens, there are many different user groups who use and experience our transport system and networks in different ways.

I recommend that an inclusive and diverse lens is applied to all policy, guidelines and design materials for infrastructure projects across the ACT, including temporary traffic management plans and that user perspectives and lived experience be incorporated at all stages of design, development and implementation. These perspectives must include women, people with a disability, people from low socio-economic demographics and other minority groups.

Extensive community consultation undertaken by the ACT Government already goes some way to achieving this. However, it is important that these voices actively influence outcomes at the final stage of decision-making and designs.

Specific design outcomes and service initiatives for diverse groups and those with particular needs might include path width and curb ramps, as well as ensuring that PT options are affordable, accessible and safe at all times of the day and night.

RECOMMENDATION:

40. Establish a process to incorporate diverse and inclusive voices into policy, design and final infrastructure for transport outcomes; including the voices of those in our community who are most disadvantaged and vulnerable. This includes PT options outside of peak hours, providing access to education, medical and social support.



Schools



Graphic courtesy ACT Government

One respondent noted that the policy at her child's school contributed to car-dependence for herself and her child:

Despite having older siblings and a safe route home, the school won't allow kids in younger years to walk home without a parent.

Other respondents noted a timing and logistical challenge resulting from school drop-off and pick-up requirements:

(Riding with a child to school might be an option) but what then... and how do you (ride home together) after 5:30pm?

Some respondents stated that improved bus routes to schools are needed for students. This would help achieve greater independence and travel flexibility and options for children and parents:

The kids need to be able to get to and from school safely. Primary aged kids mean they can't catch multiple buses through hubs. Installing school buses again would be wonderful and enable the whole family to utilise public transport.

Costs associated with before and after school care were said to create time pressure, meaning that parents' only viable choice to get a child to school or childcare, and themselves to work/office is via private vehicle.

Some comments also noted a misalignment between school hours and the number of annual school holiday weeks, with the reality of full-time work for single-parent households and dual-income parents, making daily household schedules challenging:

(We need a) serious revamp of school timings and school holidays... It's time to get rid of the antiquated school system we currently have and bring in something more suitable for the 21st century and the reality of 2 working parents.

Private vehicles are considered to offer an advantage for parents who are concerned they may need to pick up children unexpectedly during the day. It was felt that PT and active modes of travel do not offer a sense of confidence in this situation.

The 'Her Way' survey results indicated that schools and childcare centres are critical points in many people's journey to work. The logistics for parents involved with school and childcare drop-off and pickups are significant. Added to this is a concern held by many parents about the safety of children walking or riding to school alone.

Studies have shown that when children ride or walk to school, they develop greater confidence, independence and resilience. They are more likely to be alert and attentive as well as physically and mentally healthier than peers who do not walk or ride.

Studies have also shown that children are often keen to walk or ride but are prevented from doing so by concerns held by their parents.

The ACT has an excellent Walk and Ride to School Program, facilitated by the Physical Activity Foundation in conjunction with the ACT Government. This goes a long way towards encouraging and enabling AT for school children.

School-day hours and the number of weeks of school holidays per year were developed at a time when many households had one parent at home either part- or full-time. It may be that there is a more contemporary approach to assist households meet their children's school requirements while also meeting the costs of living and a quality of life. This could be a long-term option for the ACT Government to investigate.

RECOMMENDATIONS:

41. Review school and childcare policies to ensure consistency in approaches for young children to travel home; including addressing the appropriateness of having older siblings or others accompany them
42. Continue to encourage and facilitate school travel by PT and AT
43. Explore options and demand for Park and Ride at schools in Murrumbidgee; a pilot project could be undertaken at one or two schools in Murrumbidgee

Conclusion

Respondents of the Her Way survey are willing to make changes to their travel behaviour and modes of transport only if it is perceived to be – and is actually – easy, convenient and safe.

There are actions that can be taken now – low-cost, minimal intervention - and actions that will require a longer-term, more resource intensive approach that will help women living in Canberra's south, who want to make changes to their mode/s of transport, to do so.

My actions

I will be working with the community in my electorate to do what I can to assist and support women and others to incorporate greater use of PT and AT in their daily and weekly routines. I will be using opportunity through my role in Assembly Business to progress initiatives related to supporting more women to ride bikes for short journeys and work commutes. I look forward to continuing to work with Minister Steel – particularly during the period of disruption during light rail stage 2 construction – to implement outcomes to help ease traffic congestion and ensure smooth commutes.

I will also continue working closely with my community and stakeholders to support and assist women and others who want to change their travel behaviour.

And I am committed to making changes to my own travel behaviour, to catch a bus or ride a bike when it's convenient and feasible.

Thank you to everybody who has provided input to inform this Recommendation Report. I hope you see some of your comments and input reflected in this document, and that it presents a practical and pragmatic approach toward making greater use of PT and AT, and less reliance on private vehicles.



Key references

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https://www.researchgate.net/publication/235356404_Cycling_Getting_Australia_Moving_Barriers_Facilitators_and_Interventions_to_Get_More_Australian_Physically_Active_Through_Cycling

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APPENDIX A 'HER WAY' - COMMUNITY COMMENT AND INPUT

Quantitative analysis from online survey results

Pie graphs indicating quantitative responses are available at Appendix A. In summary, quantitative analysis from online survey responses have identified that:

- Most respondents (60%) travel by private vehicle as driver to and from work, followed by bike riding (15%), a combination of modes (9%) and public transport (7%). Only a small number walk (6%) or are a passenger in a private car (2%);
- For non-work related journeys, most respondents (71%) travelled by private vehicle as driver (an even higher percentage than trips to work); followed by being a passenger in a car (11%), riding a bike (9%), walking (9%) or a combination of modes. No respondents indicated public transport as a mode of transport for non-work related journeys;
- Just under half of respondents indicated they would like to make better use of other forms of transport, with a further 26% indicating maybe/depends. 19% indicated they didn't want to switch modes of transport. A small number were unsure if they wanted to make change;
- Of those who are interested in using other modes of transport, the main purpose would be for journeys to or from work (38%), followed by journeys for recreational, sports or social purposes (27%). 24% respondents indicated they would switch modes of transport for journeys to or from the local shops, and a small number to take children to or from school.
- Of those who indicated their interest in switching modes of transport, 40% indicated interest in riding a bike, 37% indicated interest in public transport and 19% indicated interest in walking. A small number expressed interest in scooting (2%). Only one respondent indicated she was interested in making greater use of private vehicle as either passenger or driver;
- 69% of respondents work full time, 22% work part-time; and the remainder are either not working, undertaking study or are casual workers;
- Flexible working arrangements are available to 51% of respondents; 23% indicated that partial flexible working arrangements are available; and a further 24% stated that flexible working arrangements aren't available to them;
- Of those for whom flexible work arrangements are available, 71% utilise these arrangements while 29% do not;
- 45% of respondents would like to work more flexibly; a further 26% indicated maybe/depends; 28% do not wish to work more flexibly; and one respondent was unsure;
- Of those who indicated they would like to work more flexibly (including maybe/depends), 54% would like to work part-time from home; 21% would like more flexible hours; and 12-13% would like to work full-time from home, as well as a further 12-13% preferring to work from a co-working space closer to home, either full or part-time.

The final quantitative question in the survey asked respondents about the barriers and constraints which were preventing them from making the changes they would like to make. Responses illustrate that there are many barriers and constraints (respondents were able to select all that apply). These include, in order of the highest to lowest number of respondents selecting these constraints:

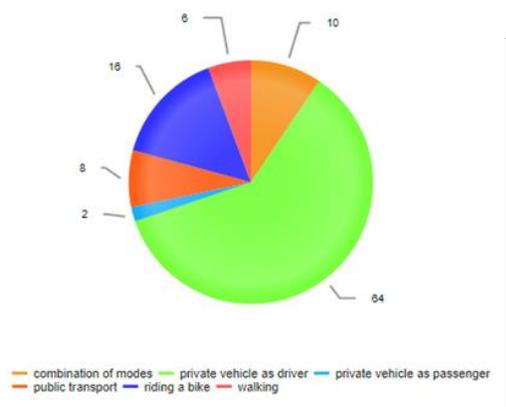
- Bus: timetable: 50
- Distance: it's too far; takes too long: 49
- Safety: separated cycling infrastructure, away from traffic: 41
- Logistical constraints: too much to do: 40
- Safety: lighting: 39
- Weather: 39

- Logistical constraints: too much to carry: 38
- Logistical constraints: need to look neat/professional: 31
- Bus: facilities/services: 28
- Safety: secure bike parking: at your workplace, the bus stop, local shops or other places: 24
- Workplace arrangements: unable to work from home: 23
- Lack of confidence riding: traffic concerns: 22
- Workplace arrangements: rigid workplace times: 19
- Unsure about the route (including bus timetable): 15
- Workplace arrangements: difficult to request other arrangements: 15
- Workplace arrangements: lack of end of trip facilities (showers, change rooms, bike parking, lockers): 14
- Bus: ticket pricing: 13
- Lack of confidence riding - skills or fitness: 11
- Lack of access (including financial constraint) to a bike or scooter: 9
- Image and identity (ie - you don't consider yourself a bike rider/cyclist or as someone who would catch public transport): 3

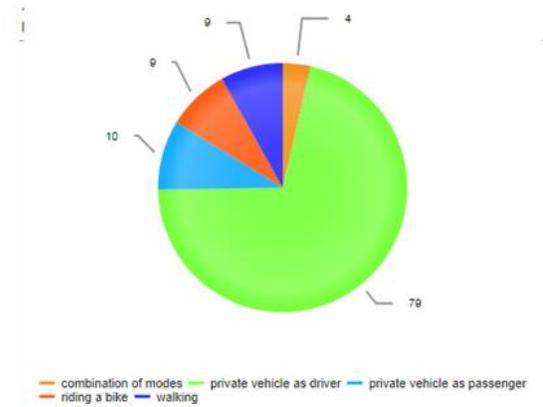
**Please note that respondents could select more than one option in response to these questions*

Quantitative Analysis pie graphs

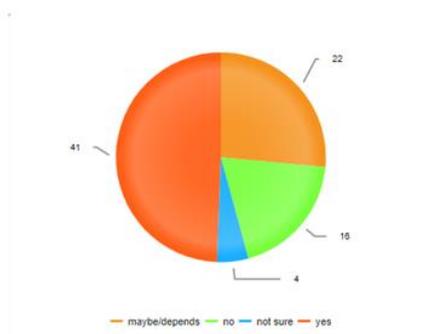
What is your main mode of transport to and from work



What is your main mode of transport for non-work related journeys?



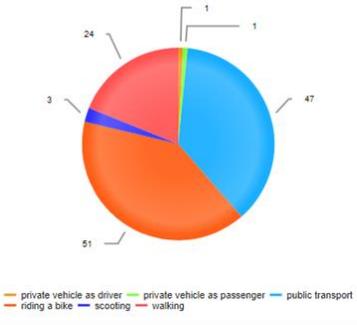
Would you like to make better use of different modes of transport?



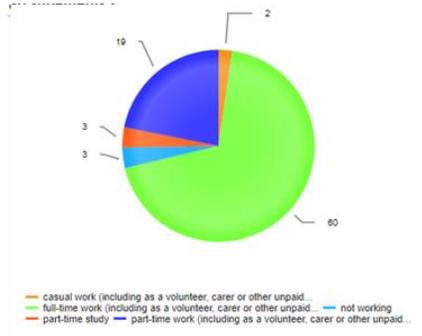
If you answered yes or maybe/depends, what types of trips would you consider switching modes?



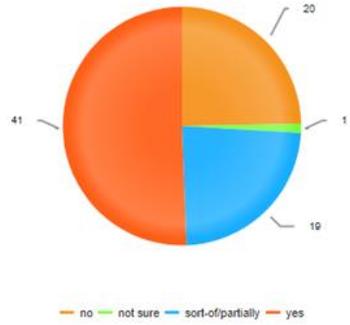
If you answered yes or maybe/depends, what mode would you like to make more use of?
Please tick all that apply



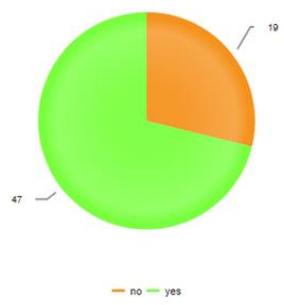
How would you describe your current working arrangements?



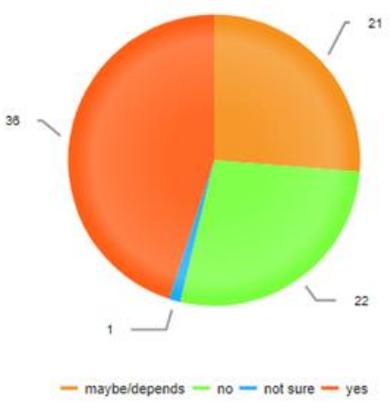
Are flexible working arrangements available to you?



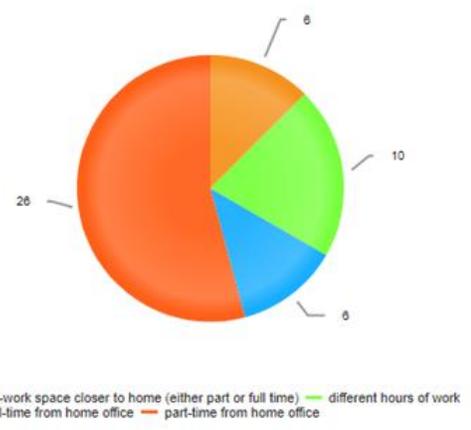
If flexible working arrangements are available to you, do you currently work flexibly for part or all of your working week?



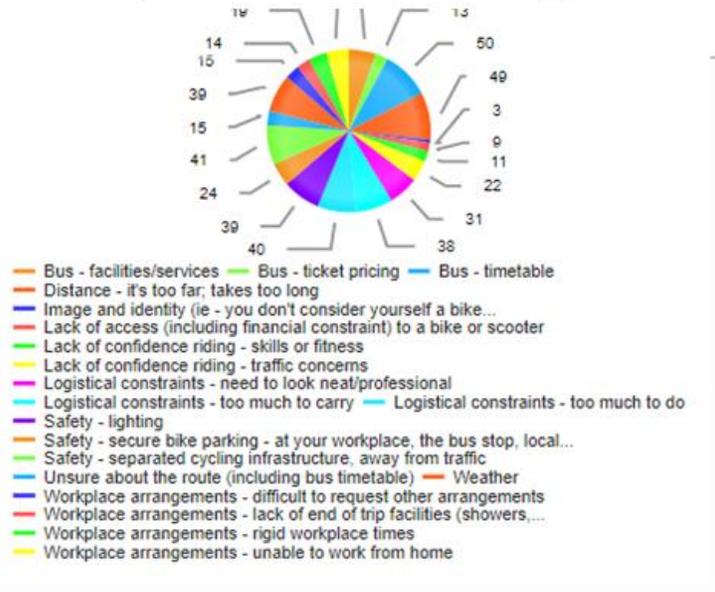
Would you like to work more flexibly?



If you would like to work more flexibly, in what ways?



Please tick any of the following barriers and constraints that are preventing you from switching modes of transport



Qualitative analysis from online survey and email and social media commentary

The following summary provides insights into some of the qualitative information provided through online surveys and other email and social media commentary, grouped into key themes.

Public transport (PT) – travel times

- PT would be 1.5 hours compared to a 15-minute drive
- PT network is not flexible enough to attend sport or other activities out of hours
- PT doesn't operate late enough or regularly enough for the hours I work
- I would LOVE to catch the bus or train to and from work but the enormous deviation makes it prohibitive
- I just want to get from A to B in a fair time – I don't need to tour my and surrounding suburbs
- More frequent buses is the big one.
- There is no way I can rely on PT to get my child to school and then go to work. It would take about 3 hours on buses and would be almost as expensive as parking
- PT would have to be timely, affordable, convenient and safe, preferably with readily available toilet option
- Bus takes 45-60 minutes to get to Civic and I live just a few km from Civic. I could almost walk there faster than the bus. I would never rely on the bus for anything in Canberra.
- Bus timing is critical for us during commutes (with school children)
- PT is very expensive and not convenient enough to justify the cost and wait times
- I work in a location with very limited bus routes; I work rostered shift hours and need to pick up my baby from childcare – It's nearly impossible for me to take PT.
- More frequent buses are needed
- Buses are too infrequent and poor routes; it's almost faster to ride
- The bus routes currently do not link up at anything approaching a reasonable time for either my partner or myself to get to work. There is no bus stop within a convenient walking distance of our house that goes to either of our work; there is no connecting bus to get my partner to and from work that is reasonable (either because the connecting stops would make them late for work, or because it ends up taking at least an hour via public transport, for a trip that only take 15 minutes driving directly).
- It takes 20 minutes to drive or, ridiculously, 1hour 5 minutes on a BUS!
- Public transport takes at least twice as long as driving to work
- The time to get to work on public transport is impossible on top of my long days. It's over an hour from my house (Fadden) to the city. I can't do that, meet my work requirements and pick up my kids on time.
- buses take 3 times as long as driving
- The other options by bus go via Tuggeranong, Weston or Woden and take well over an hour. I can't spend 2-3 hours a day on public transport when it's 40 min return by car- I have kids and other responsibilities.
- The public transport changes left me with a 1hr 45min bus trip, or a 5-10min drive, so I drive now.
- It's quicker for me to drive than catch the bus to work
- maybe if there was an actual public transport system... that wasn't dangerous, dodgy and slow... ie taking 1hr instead of 10minutes
- Public transport would need to be quicker, including via schools where I do drop offs
- As someone who desperately hates to drive, I have no choice because the public transport is so bad it would take me double the time to catch 2 buses to Civic than it would to drive in peak hour traffic (which is already 48 minutes).

- I'd happily take the bus, and I did happily take the bus. Until they got rid of the direct to city services I was using. Time with my family is more important to me and I miss that time with the current bus network - I would spend nearly 3 hours a day commuting if I went back to the bus.
- this is the real issue with catching public transport in Canberra for many. Increased commute times from the changes means too long spent on public transport.
- And a bus that doesn't take 4-5 times as long as driving. Good luck.
- I'm all for light rail where it makes sense, but you aren't going to get people onto public transport from the south if it takes twice as long as a car. Maybe an ideal opportunity to try a dedicated rapid bus route. Light rail will also need overtaking at stations so they can offer a rapid light rail service. People value their time in Canberra, it's one of the main reasons to live here.
- Every minute of every day is planned and precious. We have no choice but to choose the most efficient way to do everything.
- The time of the commute using public transport is not worth it when I can save 30 mins each way by driving and avoid a lengthy walk to the rapid bus stop. Timing also doesn't work for picking up and dropping off children at multiple locations (day care and school) - I would have to walk home and get the car and then drive to get them. When you factor in the above, and it only costs \$5 extra than the bus for parking each day, it's not worth the effort. Prior to having children I caught the bus to and from work each day, but this was before the bus timetables were changed. Back then I had two bus stops with rapids to the city only 3 mins walk from my house. Now it is a 15/20 minute walk.

Public transport - routes

- More frequent and direct bus route through Hughes to Civic. I can easily park behind the Hughes shops (near the school) but the bus from there is infrequent and takes 35 mins to Civic (via Yarralumla!) instead of 20 mins to drive or catch a Rapid from Woden.
- A school bus route through Lyons to Hughes Primary School
- I feel Kambah is largely ignored when public transport planning is done. There is low ridership because the options are inconvenient and unreliable, and because ridership is low, options keep being cut back. I appreciate some people will always have to travel by car, but the current system does not help.
- electric buses with better routes and speed that reach deep into suburbs would be better than a tram
- Need a bus stop near Yarralumla or Deakin on Adelaide Avenue
- Rapid bus from Garran Primary/Hospital to Barton please
- The bus timetables are awful, especially for early morning travel, with buses not linking up. Despite living southside, it is almost impossible to conveniently travel via public transport to Fyshwick and then to Tuggeranong
- Need a direct bus from Tuggeranong and Kambah to Belconnen
- People coming from the southside of Canberra onto the ANU campus are severely disadvantaged in terms of public transport and I encourage the ACT Government to explore better solutions.
- An express bus to ANU western campus and Black Mountain
- We need better and more logical bus routes and bus stops
- Buses are not well connected, they do not take me where I need to go
- We need more buses across wider areas of Canberra, servicing more people, if you really want to take cars off the road
- I would love to catch the bus but it takes a lot of time; especially between interconnecting buses. The issue is the length of the commute.
- If there was a direct route to my work, I'd consider it! And if I didn't need my car to go from one job to the next I might also consider public transport.

- Commute diversions - Rapid buses don't go to a number of local shopping centres, and Interchanges are a fair walking distance from bus stops local services and supermarkets in major centres.

Public transport – school routes

- Direct PT routes to school are needed
- Buses to schools need to be fixed. Although my daughter can get home quite easily from ADHS, there are no rapid routes at all in the morning, instead taking three different buses. Consequently, we drive
- I don't live southside, but do know how hard it is to get my son to school with deleted bus stops and no school route to his school, so I drive him, which then means I have to drive to work or I would be hours late.

Public transport - safety

- PT doesn't feel safe travelling alone at night
- Bus stops need better lighting
- In the evening, it doesn't always feel safe waiting for a bus on the street in the dark
- There is so much the Government could do to improve PT for women. Changing buses after dark at the Woden bus stop is very dangerous. There is a staff kiosk there but in my experience, no one has come out to help. We need better lighting and a security guard visibly helping the people waiting for a bus. Similarly, you need to review lighting after dark at bus stations
- Safety is biggest concern, as much of my travel occurs at night-time. It's not just about lighting, but also the amount of other pedestrians (usually limited) and distance from public transport stops to home.
- feel unsafe catching buses after dark
- bus stop near my home requires me to cross a busy street or go through a very dimly lit underpass
- The Woden bus interchange is not a safe place to be in the dark or even after high school with children roaming in groups, stirring others up
- Woden bus interchange is unsafe, dirty and unpleasant
- For women in particular it means: switching at "hubs" increasing the chances of running into danger, and an increase of walking in the dark during winter as they took away many stops and altered routes in favour of longer walks.
- When they changed the bus timetables on the Northside, I felt there was a real risk of being unsafe, while changing at one particular area (Mitchell). I rang frequently and expressed my concerns, for when I travel to family. No comprehension and no change to timetable. I'm an Aged Pensioner, not willing to take risks. Now I take a cab.
- Ok for summer, but not in the dark- when women feel safer they might. So put a bus stop closer to my house in a well-lit area, instead of asking me to walk across a dark oval and an unlit bushy path to get to a main road
- I think there is some value in offering a women and children only carriage on the light rail. Particularly when travelling at night, women may feel more safe travelling with other women. It also allows security measures to be taken promptly if a male passenger enters the carriage such as having the driver request that the male selects a different carriage, and if they are non-compliant organising for police to be stationed at an upcoming stop.
- Let's see - sitting in my safe car where I can lock all the doors if I feel unsafe, or sitting out in the open in a dingy bus stop ... nah pass thanks.
- If the ACT Government wants to increase patronage of PT they need to increase the frequency of services and provide adequate lighting for bus stops and shelters so both men and women feel safe.

- all bus stops need to have more lighting and safety measures for the winter months. A lot of people leave work in the dark!
- the solution to that problem isn't to just not use public transport. It's to make it feel safer.
- better public transport is great, but if women don't feel safe using it, they won't.
- The vast majority of anti-social, threatening, and violent behaviour I witness on our buses and trams comes from <18s (children).
- Interaction with other passengers on busses can be intimidating
- Addressing the barriers to why women do not use public transport is well beyond the scope of what this campaign would be able to deliver. The safety element alone of public transport or cycling/scootering/walking has long been a concern for many women in Canberra, especially in our winters. While the statistics of assault may not match the degree of concern, I know I have been 'on alert' at bus stops in early evenings and then avoided walking through places such as Telopea Park by myself. Does your campaign intend to resolve these safety issues for women and men?

Public transport – facilities; technology

- Bus shelters don't offer much protection from the weather
- Working late, it's very dark and cold in winter and I don't want to take PT
- It would be really great if Canberra had a bus transport app so commuters could check on the status of the buses and when the next one is arriving at their stop. It would mean you could wait in your home or office until it is time to make your way to the bus stop, and safer.
- Invest in more reliable bus schedules – often the app that is supposed to give live updates isn't correct.
- More information about public transport options
- I live in Kambah and work in Lyneham. I would like to take public transport sometimes. There is one bus (the 181) that goes from here to the city directly and it only runs 3 times a day. The last one is the only suitable one for me. There is no timetable to say when it will be in Kambah; it only says when it will be in Gordon, then when it gets to the city. If I guess the wrong time and miss it, there aren't more coming. It's too risky to try to take the bus when they run so infrequently and the schedule is opaque.

Schools and trip-chaining

- We need a serious revamp of school timings and school holidays. Kids should have a longer day at school which allows parents time to commute by PT... It's time to get rid of the antiquated school system we currently have, and bring in something more suitable for the 21st century and the reality of 2 working parents
- I cannot risk the school calling in an emergency and I'm left to rely on public transport
- Knowing that I might need to collect a child from school etc with little to no notice makes other options unattractive
- Bus times too infrequent and too crowded I need the flexibility to be able to leave unexpectedly as I have a primary school age child
- I have to have ability to pick up children in time
- We need buses that work with school before and after care
- Consider changes to before and after school care hours. When you have to do drop off and pick up – particularly with big fines if you don't make the 'after' cut-off at 6pm – is challenging.
- The speed between kid drop off and pick up and getting to/from work is a big one.
- It is impossible for a working mother to use public transport and do school runs with no infrastructure to support them
- I can't manage to pick up kids at day care and kindergarten other than driving, for timing reasons. Despite having older siblings and a safe route home, the school won't allow kids in younger years to walk home without a parent.

- The kids need to be able to get to and from school safely. Primary aged kids mean they can't catch multiple buses through hubs. Installing school buses again would be wonderful and enable the whole family to utilise public transport.
- families can't get their kids to school in Hughes from Lyons without driving (eg if their kids are still young). And if you ride with them, then what? And how do you get home after After School Care at 5.30pm? This government needs to stop banging on about "active travel" as if it's some sort of lazy lifestyle choice of parents to drive kids to school, when the PEA system, school capacity issues and unimaginative bus routes make driving the only reasonable option.
- In the morning I have two drop-offs: one to day care in Calwell and one to primary school in Gowrie... It requires a journey back to the interchange to swap buses. I'd prefer to avoid the interchange with small children in tow. A car is warm, fast, safe for small kids and convenient. I would have to pay more for before and after school care for the time it would take me to use PT
- As a cyclist, I heartily encourage cycling if you can. As a mum and a realist, I know it's impossible when you have kids to drop off at childcare or school.
- I can't imagine this is great for mother's - taking two kids to school or childcare takes time and then if day care calls if one is sick! Can't see cycling or scooters are a solution for working mums
- Women often have more need to drive rather than catching public transport. As well as holding down busy jobs, they are often the ones picking up kids and ferrying them from one extracurricular activity to another! perhaps women in Canberra are less likely to use public transport because a) it is inefficient b) they have to drop off and pick up kids from school (Barr decision re school buses) c) they have active kids who do extracurricular activities rather than play computer games.
- I wonder how many of these women you want to use public transport drop kids off at school first? With the current timetable these women will be getting to work just in time for lunch!!! If you really want more people using public transport how about better express services to/from the southside? An hour trip from Civic to the Tuggeranong suburbs is ridiculous, people are already time poor, the current system uses a total of 2 hours a day of that time.
- Space - Where do you put kids on a packed commute bus.
- I have 3 kids across 2 different schools (preschool at a different site and primary school) and need to do drop offs/pick ups and get to and from work. It is entirely impractical to think I can change my mode of transport. This survey is missing the point entirely.

Other trip-chaining and responsibilities

- The work commute is not a women's issue, it's a family issue.
- Women often bear the load in taking children to school/childcare/after school activities/ errands on the way home. If public transport was a valid option, many women (and men) would already be using it.
- Considering that women are, most of the time, the primary caregivers - a job which entails dropping kids off at school and other activities, while also performing a range of other domestic duties that require leaving the house - shouldn't the spotlight be put on men to catch public transport?
 - Why should women be required to take more time (because the tram will take longer than driving) to perform duties that they don't get paid for?
 - Not to mention the additional labour that comes with ensuring that errands and schedules align with the tram timetable.
- So in an article that recognises many women do school drop off/pick ups and grocery runs, it asks that we also now do that on public transport to lessen traffic congestion? If women do the majority of this, wouldn't it make sense the parent not doing this and going to and from work, be the one on public transport?

- Marisa's pro-tram, pro-bicycle thought bubble surprisingly (or not surprisingly) fails to acknowledge the practicality for women of a car to transport children, do the shopping, visit friends, attend medical appointments, go on holidays etc
- How much more time would mothers have if they had one less side trip to make between work and home? At what age is it reasonable to expect children to be able to make their own way home, and perhaps stop at a friend's house where there is a work-from-home, part time working or full time housekeeping parent to supervise until one of the parents is available to pick them up? What's stopping the father from taking responsibility for even one or two school shuttle trips a week?
- These aren't discussed as strategies for enabling active travel. Instead the suggestion to mothers is to add squeezing a bike into their car to the list of things they already do as part of their commute, shopping and kid-ferrying activities. That sounds quite counter-productive to me, and in fact I'd suggest that it's basically setting women up for failure.
 - Number one tip that any productivity consultant is going to give you is that you can reduce your workload by delegating tasks or simply removing tasks from your list.
 - Do you really need to drive 12yo kids to school? Can't they walk, ride or bus themselves?
 - Does it really need to be you transporting the kids? What about arranging a round robin with parents of your child's friends in the neighbourhood?
- The fair division of labour in the household is the front line battle that we need to fight, with all the other battles dependent on this one.
- But from the research which they cited these results, society would benefit more from men catching the bus (not women as the article pushes for) while the constitution is going on
- I need the flexibility of being mobile to fit in errands around the family schedule. Everything has to be planned out to be efficient.
- Challenging to get shopping home without a car

Pricing

- Affordable options are critical, especially for single-parents
- Another option might be to include travel time into your paid work day for people who use green transport - I can drive to work in 20-25 minutes but if I bus at non-peak times (when I can sit) it will take closer to an hour. It just isn't feasible.
- People with primary school age children should get parking subsidies - it is almost impossible to combine school drop off pick up with public transport.
- Public transport in Canberra is much too expensive for adults without concessions. It shouldn't be about income/concession, but about incentivising the use of public transport. I'm not going to use the bus, especially in conjunction with riding part way, if it's similarly priced to driving my car. If it were cheaper I would ride a bike part way and use the bus the rest, but it currently isn't worth the time, effort or cost to do so, and unless it can come down below \$3 a day (return) it will remain too expensive.
- There should be incentives to catch a bus on rainy days if you usually ride
- Currently it is cheaper and time saving to drive and pay for parking than commuting in the morning
- We need subsidized purchasing options
- it is much easier for a middle-class public servant woman who has full time employment to buy a quality bike, maintain it and be "easily" using it to and from work because most of the ACT Government buildings now have access to end of travel facilities. However, this is not the case for women working in the community sector who are significantly lower paid and often located in old and poorly maintained buildings. The issue is even worse for the unemployed, or those in precarious employment who probably need access to affordable public transport more than those who can afford to buy a car, a bike or a scooter but "choose" to opt in to active-travel.

Multi-modal

- Additional park and ride or bike and ride options for the southside. The closest one to home that I could use after my first drop-off is 10km away
- Change the park n ride rules to enable me to ride one way.
- More spaces in the park and ride at Woden. By the time I drop my child at school (earliest is 8.30) then get to Woden, the park and ride is often full, no doubt with people who haven't had to drop off school kids. I also suspect a lot of people park there but don't catch the bus. At least post-covid there are sometimes still spaces at that time of day, soon it will be back to the pre-covid situation of being full soon after 8am. There is a lot of parking next to the bus interchange but only a tiny bit is for park and ride.
- Free parking on the south of the lake and shuttles to the city by road or across the water could be a great option to removing congestion if time efficient.
- Need improved 'last-mile' PT. If I drive to work it takes 20-minutes. If I catch the bus, it takes 1:15-hours, mainly due to the walking at each end.
- I would like to go back to bike commuting, like I did when I lived in Weston Creek. But there is a geographical challenge if you want to bike from the Tuggeranong valley - you can't be just a casual cyclist to get up the big hill to the city, you have to be pretty serious. Put on more arterial buses and I will happily ride to the bus stop halfway in Mawson or Woden. If i can be sure I will actually get a seat on a bus.
- My kids and I rode into Civic and returned home using the tram earlier this year and I had to be separate from 2 of my kids because all of our bikes couldn't fit in the one bike area on the tram. It's not a big deal by being 25ms away from a 5 yrs old on a moving tram is somewhat disruptive to other commuters. Also, if people are not confident getting bikes onto trams or are sensitive to disrupting others, this may be a barrier to mixed mode travel.
- when I do catch an express bus I still have a fair way to walk to the express bus stop, and I have to do a very long day due to lack of buses on the express route
- I am also really keen to try using the bus and taking my bike along for the ride. Wish this was free and also am not confident putting my bike on the buses bike rack.
- I've tried leaving my bike at Garran primary school and catching the bus but it takes FOREVER.

Bike riding

- I need more fitness if I was going to ride a bike... maybe an electric bike!
- I work too far to cycle and would feel unsafe, especially after work
- Setting up bike buddies would be good. I ride frequently; I'd be happy to meet someone once a week to show them the route and help gain confidence on the road/paths
- Riding a bike has a logistics overhead and time impact that I can't fit in
- I used to bike more but health and work reduced my capacity and it has been hard to get back into it
- I'm afraid the other deterrent to more frequent bike use for me is the helmet requirement
 - not only does it lead to unflattering 'helmet hair'
 - also the helmet must be lugged around the shops etc with you which is a disincentive unfortunately.
 - I appreciate the safety considerations, but I do feel that on a gentle bike path commute or short trips on flat terrain it could be discretionary, as it is in every country in Europe.
- I need help and guidance on selecting a bike
- Clearly the ACT weather is not conducive to riding in winter and summer
- Unable to ride to work because I have to carry laptop and charger please notebooks on bike and they are too cumbersome
- I'd have to ride at 630 or 7am in the morning to avoid traffic which makes is less appealing.

- Commute distance too long to cycle
- It takes too long to bike from Kambah where I live into the City where I work, and I don't want to bike on a road.
- As a woman I ride my bike, but I'm not confident I know how to repair a tire should I get a puncture. And I don't know how to care for my bike.
- Easier ways to determine how I can ride safely to work from Tuggeranong to Bruce.
- And of course 20kms each way is very time consuming, especially if you have to pick up kids/shopping
- What if it's dark when I'm leaving home in the morning or returning at night? Will I be safe? Will I feel safe?
- Safety- would I feel safer on a bike than in transport? I can move seats but I imagine cycling with kids against cars will feel scary. Plus riding on a lit bus can be less scary than riding in winter when it gets dark.
- Also, magpies. There is a horrible one in Yarralumla that always swoops me. They are a deterrent for anyone cycling. I know a lot of people who don't cycle until December because of this. Perhaps it is time to do a new survey of magpies in Canberra to determine whether they really need to be protected?

E-bikes

- I'd like to try an electric mountain bike, but it's still a long trip.
- It's a long commute up a big hill to my work Curtin > Bruce. I'm on the wait list to borrow an e-bike from see change. I want to borrow one and see if my commute is doable with an e-bike perhaps 2 days a week (I work 4)
- I have recently acquired an electric bike, with the intention of using that more for short trips around town.
- I'm interested in buying an e-bike but want to trial one first to test the commute to make sure it's right for me. The current queue for the e-bike library is too long and focused on the inner north
- I'd like to see affordable, quality e-bikes and scooters
- In China, there was also more tolerance of various e-bikes that had higher power levels than is tolerated here
 - they had to be registered and there was an upper speed capacity tolerance
 - it meant older people unable to drive were mobile and less isolated. I regularly saw 80-90 year olds getting around on e-bikes. An elderly person wouldn't be able to do that here because
 - a) the infrastructure does not afford them enough protection from the road and b) the only e-bikes allowed in Australia require you to put in pedalling effort to make the extra power kick in. You are not allowed to have an e-bike that is powered requiring no pedalling even if it is speed limited to 25kph.
 - Arguably those types of e-bikes (that are banned here) would be fine if we had separated infrastructure to the point that you did not ever need to ride a footpath or bike path.
 - Our out of date regulations (that have not kept up with new e-bike technologies), lack of infrastructure and poor culture toward cycling is getting in the way of uptake even in cities like Canberra where it should be so easy.
 - Good to see we're trailing the e-scooters though. Need to go further especially on e-bikes.
- I believe that more charging points for e-bikes would also encourage people to ride.
- Choose CBR = \$2 million. Could've bought 400 decent electric bikes with service costs for disadvantaged women.

Road space

- Priority should be given to bike riders – my local shops has a very difficult entry point for walkers/riders. It's great for cars but not pedestrians.
- There could also in my view be more done to introduce traffic-calming measures, like speed humps and chicanes, particularly in the inner city suburbs where daily traffic is heavy and speed limits largely ignored, to make it safer for people on foot
- Better separated infrastructure for commuting by bicycle with a child is needed, as well as greater enforcement of the 1m passing law.
- Either slow shared streets (30kph or less) or modes separated by speed – ie separate walking, cycling/scooting, driving
- Dedicated bus lanes
- I'm for any proposal that removes pushbikes from our roads.

Road use culture

- A need for greater public communication efforts to change the road culture toward cyclists
- Reckless, inconsiderate and/or abusive cyclists who verbally threaten to run me down as a pedestrian on the shared path make me feel unsafe
- there are a lot of aggressive cyclists out there on the bikes paths - 90 to 100 kilos at 30 kms an hour hurtling towards you!
- A generalisation but drivers attitudes to bike riders is appalling and very dangerous.
- It can be done, but it takes a huge policy commitment to changing not only the culture toward cycling and cyclists, but the supportive infrastructure and the regulations. I lived in China - one of the most densely populated places on the planet - and they have proper separated cycling infrastructure on every road in the capital city. Not just rural areas where there is more space, but a commitment to separated infrastructure even in the major cities. And a culture of drivers that respect cyclists.
- Paths must be shared with fast-travelling cyclists, who pose a real threat to pedestrians.

Bike lanes

- I have been riding on and off several times a week down Adelaide Ave for the past 3 years. Apart from my fitness and child school drop off constraints, one of the 2 biggest things that would help me would be improving the cycling crossovers on Adelaide Ave (in particular northbound at Yarralumla and southbound immediately after Parliament House - in both spots it can be a long wait to cross and would deter any unconfident riders)
- Separated bike path on Adelaide Ave
- I regularly ride but avoid bike lanes on roads – dangerous, too much traffic, and cyclists are not respected
- Improve the bike paths and driver education. I risk my life riding every day
- One word... Safety. Whilst the bike lanes are mostly adequate (with the exception of some that disappear) I still don't feel totally safe to ride to work.
- In terms of cycling more, the infrastructure in Canberra is not completely thought through. If I take the cycle lane on the road, it randomly disappears with no way to get to a separate bike path. There are a lot of places where the footpath/bike path meets the road with a hard gutter instead of a sloping kerb which is not bike friendly (nor is it friendly to strollers or the disabled). Especially with kids in a trailer, the cycling infrastructure is often not safe enough and does not inspire confidence.
- Safe bike lanes east to West. I work in Symonston and the bike lane stops at a cliff on top of red hill in an 80k/hr zone, and the back route through the parks is steep and scary.
- There are pretty much no safe/well-lit roads to ride a bike to Fyshwick/Tuggeranong

- My main concern is the section of the cycle path that is parallel to Callum Street in Woden. This is actually one of the best bits of cycling infrastructure ever built in the ACT. It made an immediate improvement to cycling this way. I understand that this is going to be destroyed when the apartments are built behind the police station. The new cycle path around the west side of Woden is an utter dog's breakfast and will be unpleasant to ride on as it's next to the traffic. If you're serious about encouraging Southside women to ride to work, can you please ensure that a suitable replacement (ie one away from roads) for the cycle path parallel to Callum Street is in place before the current one is destroyed? Cycling along Easty Street would be really dangerous as the traffic volume there is set to go through the roof when Callum Street is closed to cars.
- Other road works, especially the ones near the blue bridge at Yarralumla. The traffic lights there should make things better for cyclists in the end, but it's a choke point and disruption there will have an effect.
- There's a nice bike path along Yamba Drive and through Curtin, but I find the main roads into Barton too scary on the bike (and the alternative - going through Yarralumla and round the lake) takes too long. So here is my wish list! - rapid bus from Canberra Hospital to Barton and Civic. - separated bike paths (off the road!) along major roads like Adelaide Avenue (similar to the great cycleways between Curtin and Weston) - connect Garran to the wonderful network of bike paths through Hughes and Curtin - bike paths through Garran to make it easier and safer for kids to ride to school (with their parents!) eg. down the golf course where there is a bush track at present

Shared path network

- Cameras along bike paths would help prevent attacks and provide greater safety
- Better lighting is needed in the underpasses along Hindmarsh near Cooleman Court and the adjoining pathways
- Lighting on paths is needed as well as path maintenance
- We need better lighting and safer paths
- better lighting on arterial bike paths so I don't feel afraid to commute by bike in the evening
- Safer pathways are needed
- Walking/riding paths need to be in good condition, repaired and maintained. Paths need to be connected
- Better connected paths with better lighting
- Many bike paths are too narrow for bike trailers – my kids school paths aren't wide enough
- Improved lighting on foot paths for walking home after sunset
- More established path through Garran (eg, along golf course) so that kids living up the hill have a safe way to ride to school
- Better paths needed in Mitchell, especially for kids (kids have activities in Mitchell)
- I am always on the lookout for the perfect active travel route... but so far it is seemed a bit elusive. We lived in Ainslie previously and I loved riding the boys to school on the bike and then riding on to Barton from there. However, since we moved to Garran last year this has been more difficult, as we are now at the top of a big hill (backing on to Red Hill reserve). I sometimes ride my older boy to school down the golf course (which is a bit of a bumpy mountain bike ride) but I find it difficult from there.
- At night the lighting is extremely poor (at least in the inner south where I live).

End of trip facilities - public places

- Oddly, I have found that so far the biggest deterrent from using the bike for errands/appointments (as opposed to leisure riding) is that when I am planning a journey I have no idea as to where I will be able to safely leave the bike locked up when I get to my destination (these e-bikes are very expensive and therefore attractive to bike thieves!)

- I have looked online for a map of where bicycle racks are located around Canberra but have not been able to find one. I think this could be a useful thing to have available on the ACT Government's website and it would certainly encourage me to take the bike more often.
- Safe spaces to lock bikes up at shopping centres
- I need safety of my bike and belongings which are attached to the bike but could be removed
- More lock-up shelters for bikes, and more bike racks in shopping centres and local shops
- More secure bike parking at shops is needed, and the placement of bike racks in well watched areas
- We need secure bike-parking
- Bike parking facilities are needed

End of trip facilities – workplaces

- I would need showers and facilities to change at the end of the journey into work. User could pay a nominal fee to use city-centre facilities – could be facilitated through Pedal Power ACT or current gyms that offer shower-only membership which is affordable. Or a Government-built facility
- Then there is the issue of access to showers.
- I need easy access to showers at work place
- Better end of ride facilities at work
- better end of ride conveniences
- (If you ride socially/recreationally but not for transport, why?) Because you have to get changed before and after you ride and maybe do you hair and make-up and iron your shirt, which of course you have to carry with you.
- There should be requirements for all buildings to have end of trip facilities for cyclists

Walking

- When I have errands or appointments I try to go on foot wherever possible. I feel that of all forms of getting from A to B in Canberra, pedestrians are the most poorly served, especially in the suburbs.
 - Footpaths are not well-maintained, or non-existent, and there are many busy roads with very few safe places to cross.
- Decent infrastructure for walking – wide paths in good condition with pram ramps, priority for people crossing roads and good lighting of paths.

Diversity and inclusion

- Public mobility is a gendered issue but more so it is a class issue
- I know women, in our electorate, who are public housing tenants and their lives have recovered from significant childhood and other trauma. Hence, the confidence to learn how to ride a bike safely is not necessarily on their mind, but how to get to the shopping centre, my doctor appointment, my Centrelink appointment, my appointment with the child protection as easy as possible. some may define "ease" as "fast and affordable" some may define "ease" as something else.
- The same disparity is applicable to people who have the option to work from home or other sites close to home, and those (often in the frontline industries) which are female-dominated yet low-paid and heavily casualised industries such as teaching, nursing, aged care workers and early childhood educators, as their place of work can change pretty regularly depending on their contract from term to term or shift by shift.
 - For aged care workers, for instance, they will be picking up several casual shifts in different centres and older people's household's which can be sporadically located in different suburbs.

- Similarly, for domestic and commercial cleaners, who are frontline workers and predominantly migrant populations (and partly female-gendered) but often required to have cleaning products on them and hence the need for a private car. This experience is very different from an office-based woman who can stick with a backpack and take shower before entering their workplace.

Flexible work arrangements

- APS needs to remain flexible. A need to stop putting pressure on the return to office – by not being in civic money is spent locally at group centres
- The ACT Public Service EBA should provide more flexibility with working hours
- Need more comprehensive work culture changes toward flexible work
- WFH still needs to be more universally accepted in the public service. Covid has helped but there's still an element of guilt/fear about 'why aren't you in the office today'?
- Finishing work earlier in the afternoon so as not cycling in the dark would be helpful
- Having more flexibility makes it easier to spend more time actively travelling
- Our CEO earlier this year announced that we should all be back in the office for 50 - 60% of our working weeks.
 - While I can easily do this, the reality for me means that if I spend that extra 60 - 70 mins of time per day commuting/parking, I miss out on riding my kids to school and reading my kindy kid a story in the library before school.
 - I also have to drive to collect them from school because of time constraints.
 - So while the idea of 'showing your face' in the office is well intended, the result of such policies means that there are missed opportunities for active travel and time with yourself or your kids.
 - I obviously appreciate that there are times when face-to-face office time is essential but going to the office for the sake of being there can have impacts outside of work.
 - A silver lining of Covid for me has been more flexibility with work hours which gives me more time to actively travel with my kids and more time to spend with kids.
- I don't think I need support as such to have more flexibility around what hours of the day I work. I think it is a matter of having the conversation with my boss and negotiating a change. I feel this is more about building trust that I will do the work and managing others expectations regarding my availability.
- I need to be more assertive/proactive about my own hours
- There should be agency to agency agreements to share office space
- I would need better computer skills to work from home

Other comments

- It is not women's responsibility, nor obligation, to reduce congestion
- Seriously, the only convenient/safe way to travel is by car, so its very aggravating being told to use more environmentally friendly options
- It was incredibly infuriating and its really annoying to be told to use a bus or bike, when actually we would prefer to, and simply no longer can without it causes serious disruption and frustration.
- Love to see some positive changes... Canberra can be the best cycling city in the world!

Stakeholder input

During the period of public consultation, the following stakeholders were consulted:

- Amy Gillett Foundation
- Pedal Power ACT
- Heart Foundation
- ACTCOSS
- Aus Cycling
- Early Childhood Australia (ACT)
- Physical Activity Foundation
- Women's Health Matters
- Conservation Council ACT
- AusCycling

All stakeholders are supportive of the work being undertaken through the 'Her Way' campaign and expressed interest in offering ongoing support and continued involvement.

Key comments to help inform the 'Her Way' action plan include:

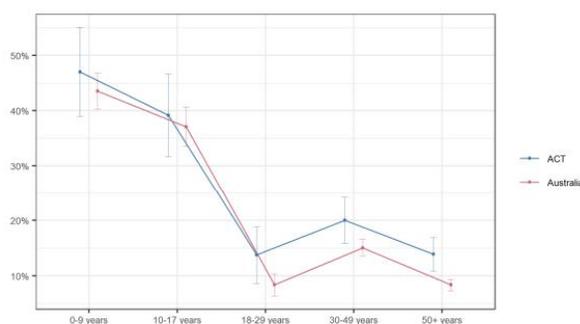
- There is lack of consistency and some uncertainty regarding school and childcare policy around young children being able to walk or ride home with people other than their parents;
- There is opportunity to better utilise PT for children's excursions, including to have them plan out their journey – this helps develop confidence in using, and normalising, PT
- Personalised six-week walking plans developed by the Heart Foundation, with individual goals, are helping 80,000 Australians this year to walk more;
- Lower socio-economic groups access transport in different ways and at different times of the day to peak hour commuters and the mass transit timetable doesn't always reflect their experience or provide for their needs;
- For those with disability:
 - the first and last mile issues are real barriers
 - Path maintenance is critical to address concerns of accessibility and to limit risk of physical injury
 - There is a need for improved 'on-demand' and community transport
 - Temporary route diversions in the shared path network can cause great apprehension, especially as what might appear a short detour can be a long detour for those with disability
 - Also, temporary route diversions can cause lack of confidence
- Issues of transport, urban planning and housing choice are closely integrated
- Live experience, across a diverse and inclusive segment of the community, is critical to inform good outcomes for transport infrastructure
- It would be beneficial to undertake an audit of real and perceived safety in public spaces across the ACT
- Existing programs assisting women to gain confidence to ride a bike include She Rides and Wheel Women;
- Other programs such as Ride Nation assist Australians to undertake more regular bike riding and build skills; There are five program coaches in the ACT;
- These programs provide close support and a 'buddy-system' or coaching programs for people who are new to riding, or are looking for support;
- Often, women feel safer and more comfortable learning to ride a bike on women-only environments;
- A key factor in behaviour change is through self-identity – a person needs to be able to 'see themselves in the picture' and influencers must be relatable;
- Grant funding opportunities might enable digital story-telling for local influencers;
- Making changes to travel behaviour involves small steps; it should not be a 'gung-ho/all or nothing' approach; messaging around small changes, once-a-week are beneficial;
- Research shows that it is often quicker to ride 7-10km than it is to drive;

- There is often discrepancy and time-lag between community desire for change and Government policy and resourcing to enable it;
- While there is sometimes back-lash to resourcing for new bike lanes ('bike-lash') - especially if it involves reduced vehicle lanes or car-parking – this is generally short-lived;
- Economic studies prove that bikes are good for business, including that access to local shops and other businesses is generally improved through reduced vehicular traffic;
- Cyclovias (streets temporarily closed to traffic, for example one Sunday morning per/month) are a proven effective method of promoting, encouraging and normalising uptick in bike-riding;
- Large-scale workplaces and employers can play a significant role in supporting staff to ride to and from work;
- Women are an indicator species for the safety of cycling in any city;
- The sequencing of infrastructure improvements and community promotion is important in achieving positive outcomes;

National Walking and Cycling Participation Survey

During the period in which this analysis was undertaken, the Cycling and Walking Australia/New Zealand National Walking and Cycling Participation Survey results were released. Key findings, of relevance for the 'Her Way' campaign include:

- Around 95% of Canberrans do some walking each week, including with mobility aids;
 - This is primarily for recreational/exercise purposes; followed by shopping, visiting a café or restaurant, walking the dog or visiting friends and family;
- Around 23% of Canberrans had ridden a bike in the past week and around 44% in the past year
 - these are the highest rates of any Australian capital city;
 - However, these rates have not changed since the last survey in 2019;
- The cycling participation rate over the past week is much higher among males (28%) than females (19%);
- Among both genders, participation rates decline as young children become children and then precipitously from teenagers to young adults;
 - The highest cycling participation rates for both genders was among children aged under 10;
- Of those who had ridden a bike in the last month, 80% had cycled for recreation and 42% for transport;
- Across the ACT 41% of residents aged 15 and over were classified as interested in riding – that is, they do not currently ride but would like to do so;
- Around 67% of ACT households have at least one working bicycle;
- Only a small minority of the population (around 5%) were classified as confident riders who take direct routes irrespective of road conditions;
- ACT residents born in Australia, North America and the United Kingdom have higher cycling participation rates than residents born in other countries;



Source:

National Walking and Cycling Participation Survey, Cycling and Walking Australia New Zealand, 2021

■ Figure 3.4: Cycling participation by age

RECOMMENDATIONS

COMMUNICATIONS

Personalised travel plans

1. Provide resources and tools to support people to prepare personalised PT and AT journey planning, including multi-modal options
2. Engage with businesses to encourage them to have discussions with their employees to develop personal travel plans

Digital technology

3. Improve communication about existing digital technology apps which provide real-time PT updates
4. Develop digital technology apps which integrate route planning, bike parking locations, real-time PT scheduling and MaaS

Marketing campaign

5. Design and conduct a dedicated marketing and communications campaign, to coincide with the disruption of light rail stage 2 construction, encouraging the use of PT and AT, and normalising this type of behaviour
6. Ensure language and imagery is relevant to the audience
7. Develop a single, dedicated website to directly respond to the concerns of women who want to better use PT and AT but don't know where to start
8. Explore opportunity to incentivise people to switch to increased use of PT and AT

INFRASTRUCTURE

Active travel network and bike-riding

9. Trial pop-up separated infrastructure in appropriate locations using low cost, minimal intervention techniques
10. Investigate options to address missing links or safety gaps in AT connections between the southside and the city
11. Provide safe cycling routes along the light rail corridor
12. Investigate the viability of direct safe cycling infrastructure connecting areas of Murrumbidgee and the existing shared path network with Fyshwick and Tuggeranong
13. Continue maintenance and repair of footpaths, and continuation of the Government's 'Age Friendly Suburbs' project

Bike parking

14. Develop print and digital maps indicating the location of bike-parking facilities across the ACT
15. Ensure existing and new bike parking infrastructure is located in areas of high passive surveillance and that they provide appropriate security for expensive e-bikes - Work being undertaken at the Woden interchange should reflect this approach
16. Undertake a review of bike cages across Canberra and the existing pass system, including their supply and demand, to explore options for greater use
17. Include representatives with lived experience in decisions about the locations of bike parking facilities

End of trip facilities

18. Finalise the ACT's End of Trip Facilities General Code and associated guidelines. Ensure these documents include the requirement for lived experience and user perspectives to be incorporated in design and construction
19. Develop best practice guidelines for retrofitting End of Trip Facilities in existing buildings
20. Assist workplaces to develop partnerships to enable staff access to EoT facilities at nearby locations where workplaces cannot provide them; and support collaborative opportunities for pilot projects

Public transport

21. Provide more lighting at bus stops and stations and popular first and last mile routes
22. Ensure light rail stops and terminals have adequate lighting
23. Ensure secure bike parking at bus stops and stations (and for future light rail stations)
24. Explore opportunity to expand the shared scooter scheme in Canberra's south
25. Explore opportunities for increased safety on buses at night-time – eg. more female drivers; more personalised services which take people closer to their home, and/or a public safety initiative similar to 'Ask for Angela' at bars across NSW
26. Consider promoting a buddy system for PT use
27. Continue to work to increase the number of buses having bike racks to give certainty for multi-modal travel

Multi-modal Woden

28. Review Park and Ride demand and capacity to ensure it is adequate
29. Incorporate appropriate levels of Park and Ride facilities with light rail stage 2 infrastructure
30. Ensure that first and last mile solutions to enable easy, seamless integration with light rail stage 2 are appropriate. Include user perspectives and lived experience in the design and development of this.

CREATING THE ECOSYSTEM

Stakeholder partnerships

31. Continue collaborative efforts with stakeholder groups for a coordinated approach to encourage Canberrans away from private vehicle use
32. Explore opportunities to assist stakeholder groups to expand and extend existing programs helping Canberrans ride bikes
33. Cyclovia/temporary street closures to cars as part of the ACTs festival calendar

Encouraging the use of e-bikes

34. Assess opportunities to encourage and promote e-bike use by ACT residents
35. Expand the e-bike library through the establishment of a Southside library to compliment that already in the north
36. Consider the provision of charging facilities for e-bikes in town and group centres

Flexible workplaces

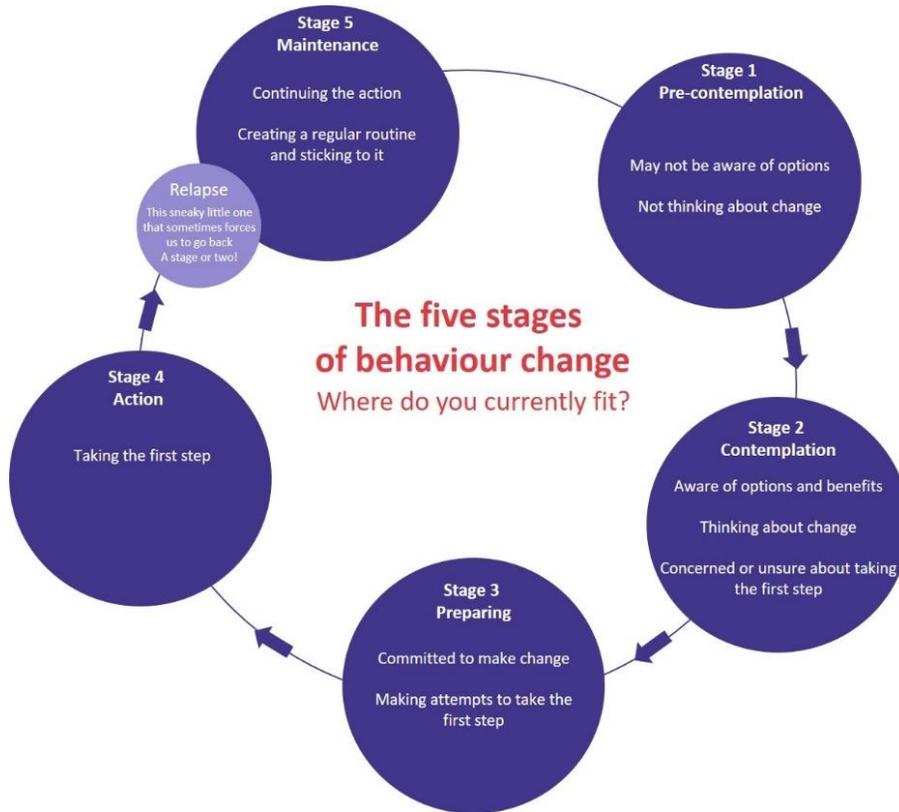
37. Evaluate and review the AT Government's Belconnen and Tuggeranong flexi-hubs, once established as per the 2021-22 Budget, to assess opportunity for flexi-hubs in Murrumbidgee
38. Engage with businesses in the ACT to encourage their employees' to engage with PT and AT travel
39. Work collaboratively in promoting a culture of flexible work across the ACT

Applying an inclusive and diverse lens

40. Establish a process to incorporate diverse and inclusive voices into policy, design and final infrastructure for transport outcomes; including the voices of those in our community who are most disadvantaged and vulnerable. This includes PT options outside of peak hours, providing access to education, medical and social support.

Schools

41. Review school and childcare policies to ensure consistency in approaches for young children to travel home; including addressing the appropriateness of having older siblings or others accompany them
42. Continue to encourage and facilitate school travel by PT and AT
43. Explore options and demand for Park and Ride at schools in Murrumbidgee; a pilot project could be undertaken at one or two schools in Murrumbidgee



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